

ROJO TRAILER



USER GUIDE

ROJO TRAILER




ROJO
TRAILER

ISO 9001
BUREAU VERITAS
Certification




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	GPN LOW LOADER		
OPERATION & MAINTENANCE MANUAL	Translation of the original guide	Rev.: 7	30/10/2023

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1.- SAFETY



This symbol will always go along the instructions to prevent material damages, personal injuries or avoid risky situations for persons.

Not following these instructions will lead to harms which ROJO won't be responsible for.

MAINTENANCE TASKS TO BE DONE ON VEHICLE

- This vehicle is a critical safety system. Please proceed carefully with maintenance procedures. A careless work may cause damages and/or accidents.
- Block lowbed wheels properly when working under the unit.
- Use solid wheel chocks under the lowbed doing works that require removing wheels.
- When removing heavy parts, fasten them on a safe way.
- Avoid unnecessary contact with oils and lubricants. Apply lotion on areas that are not protected.
- Leave a safety distance with the moving elements of the system when activating hydraulic components.
- Check that manual steering is disconnected and is kept like this when working over moving elements of the system.
- Surface of cylinder pin can be damaged easily. Damages will lead to leaks.
- To check hydraulic components, use the right tools for high pressures generated inside the hydraulic system.
- Use only original ROJO spares in maintenance and repairing works.

ENVIRONMENT

- Don't throw away used oil, lubricants or hydraulic fluid on drains, culverts or the ground. These practices are not only forbidden but also will seriously contaminate the environment.
- Used oils, lubricants and hydraulic fluids must be treated by companies authorised for its recycling. All these used fluids must be stored separately.
- Used batteries must be treated by companies authorised for its recycling.

READY-MADE ELECTRIC WIRING



It is forbidden to manipulate or cut any prefabricated electric wire to change its length. This can cause damages or interfere in the right functioning of the system.

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2.- TECHNICAL DATA

ELECTRIC SYSTEM

Voltage:	24 V
Electro pump:	2,2 Kw.
Electro pump fuse:	200 A
Power source cable from tractor truck battery to electro pump:	50 mm ²

LUBRICANT GREASE FOR THE STEERING RODS

Vehicle leaves factory with high quality lithium soap ENI type AGRIP NV/EP 2.

For a new greasing, please use the following greases or with a similar quality.

HYDRAULIC OIL

Vehicle is fitted at the factory with oil NEWTOIL HM-22. This is a mineral hydraulic oil with low content in zinc and a high viscosity to which it has been added additives against wearing, oxidation, rust and foam. This oil can adapt perfectly to extreme and changing conditions.

ISO	6743/4 HM
DIN	51.524 Part 2
DENISON	HF 0
EATON (VICKERS)	35 VQ. V104C
CINCINNATI-MILACRON	H-17672 D
AFNOR	NF E 48603 HM



It is not permitted to mix different types of oils or refill the system biodegradable oil without written authorization ROJO.

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3.- ADAPTATION TO TRACTOR TRUCK

ROJO steered lowbed electro pump needs power supply (+24volts) from tractor truck battery. That is why is needed to install an electric plug NATO VG 96 917 on tractor truck.

PLUG INSTALLATION

NATO VG 96 917 Plug, has 2 pins for connection as shown in this drawing. (fig. 3.1)

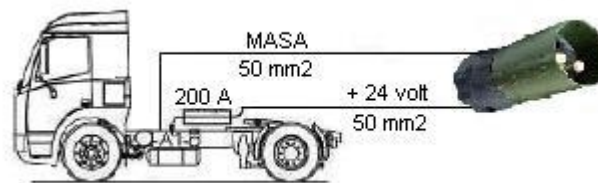


Fig. 3.1 Installation scheme

- Connect pin (+) to battery +24 volts with a **50 mm² cable** and a **200^a fuse** on the output exit.
- Connect pin (-) to mass with **50 mm² cable**.

To determine wire length, please remind that when coupling the lowbed, it will be connected to its relative plug on front side of the lowbed, together with the rest of electric and air connections (fig. 3.2).




Fig. 3.2 NATO VG 96 917 lowbed connector



Assembling of wiring system and length of the wires must allow the free turning of the lowbed avoiding that it get hooked with other components or tighten up when turning.



It is recommended to place a switch to isolate the connector when it is not connected to its plug on the lowbed.

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4.- OPERATING

4.1.- COUPLING/UNCOUPLING OF TRACTOR TRUCK

COUPLING OF TRACTOR TRUCK

- Put tractor truck closer in order to find the right alignment on the King Pin with the 5th Wheel.

If height on 5th Wheel matches with neck height, proceed to coupling under the standard procedure.

If height on 5th Wheel doesn't matches with neck height, please level neck for a right coupling height, using landing legs before proceeding.




Plug in all connections between tractor truck and lowbed.

TRACTOR TRUCK DECOUPLING



Unplug all connections between tractor truck and lowbed.

- Proceed for coupling under the standard procedure.
- Move tractor truck away completely, on a straight line.

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4.2.- LOADING



Please make loading and unloading operations with caution. When loading, respect a right sharing of loads.

Load Center Point on the vehicle LC is marked on one of the sides of the main platform by a stick. (fig. 4.1).


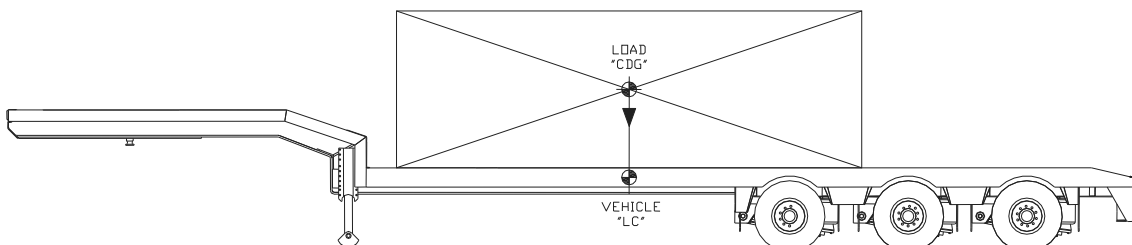
This stick marks the LC of the vehicle 

Fig. 4.1 LC stick location



To ensure a right share of the load, we have to match the load Centre of Gravity CDG with the Load Center Point LC of the vehicle.

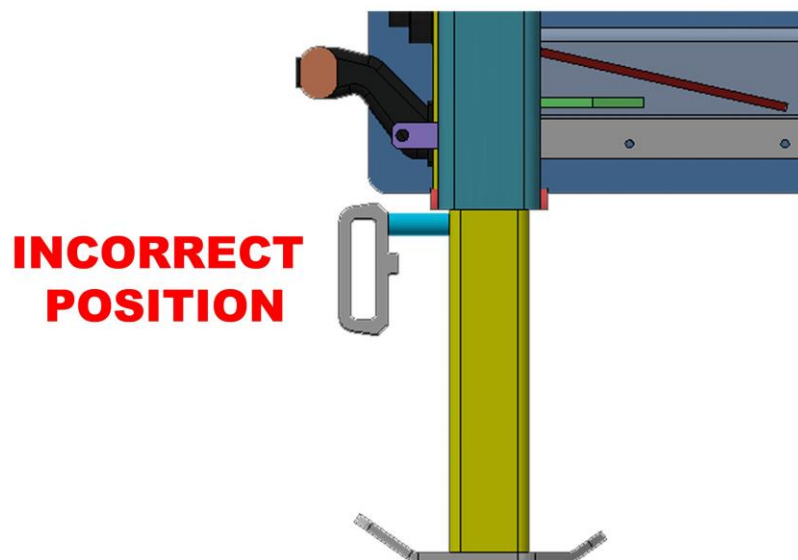
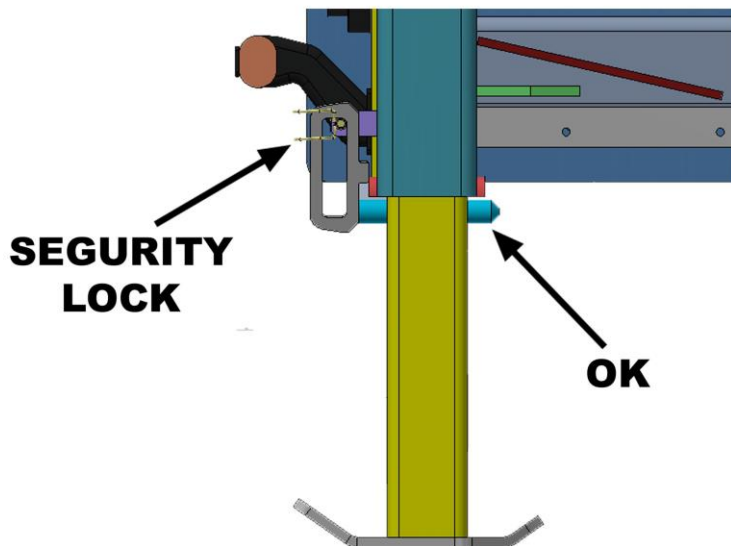
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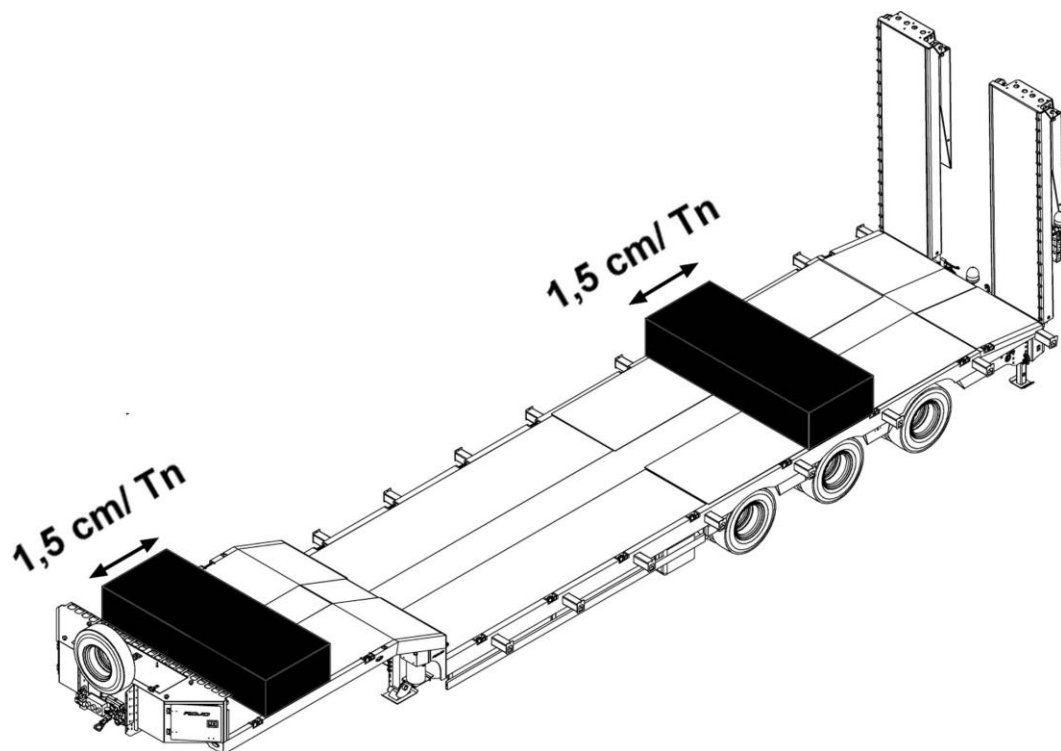
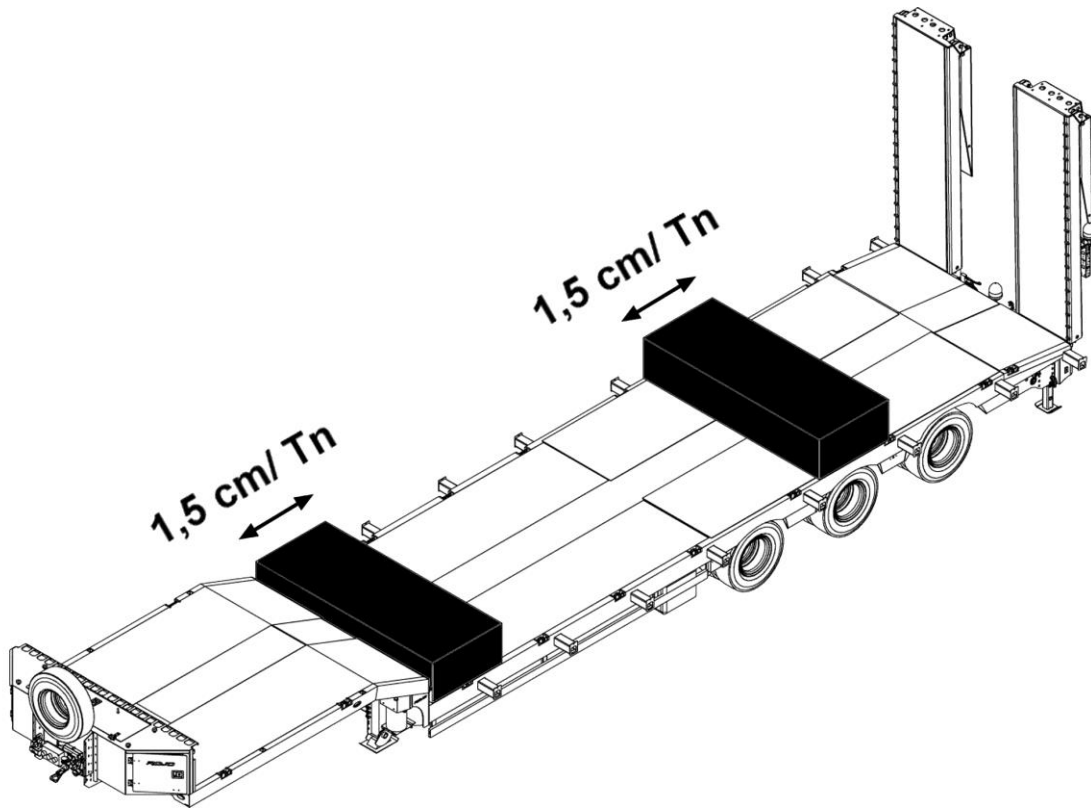


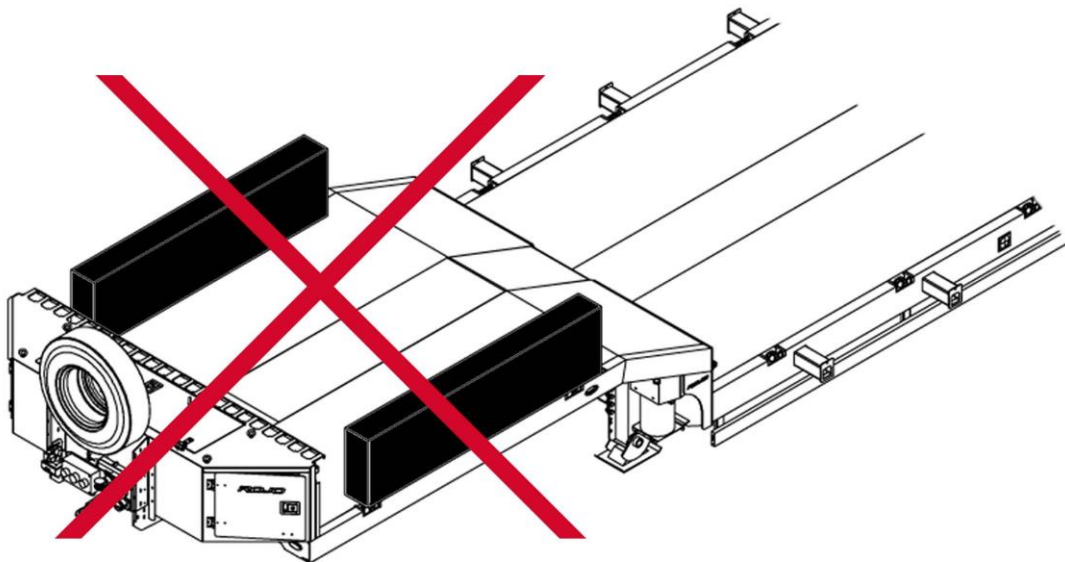
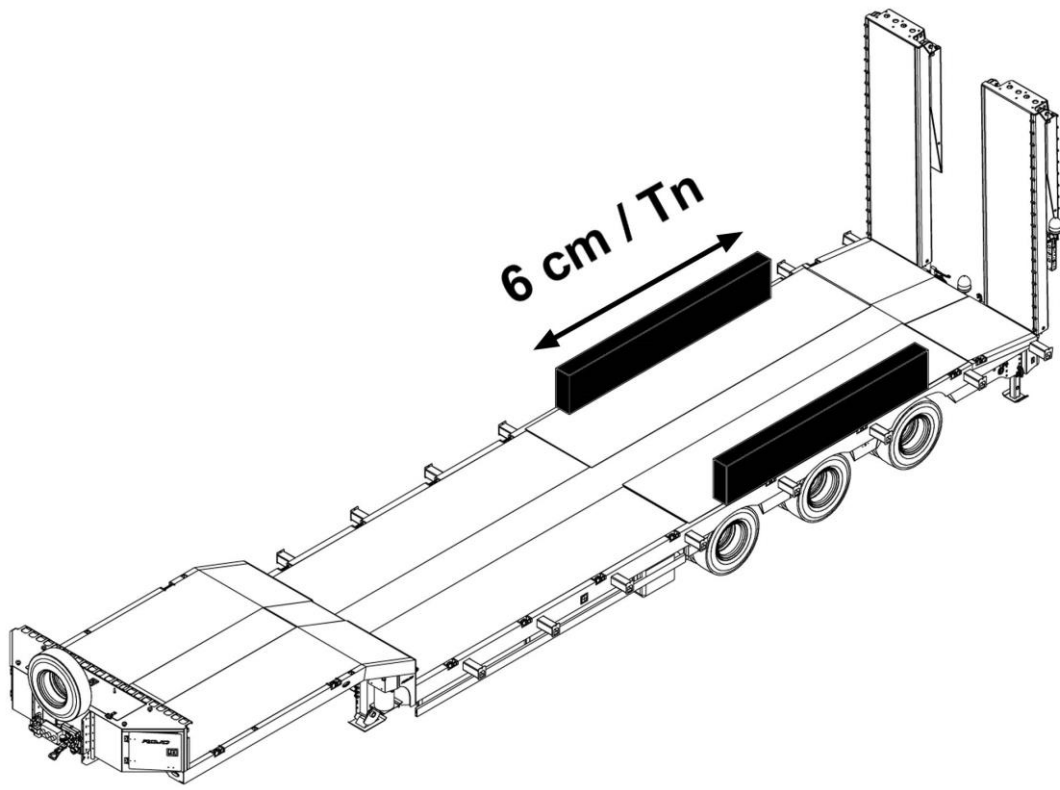
Before loading or unloading, check that the axle of the rear landing leg is in good position and secured.




If this element is not in correct position and not secured, it can harm persons or objects.







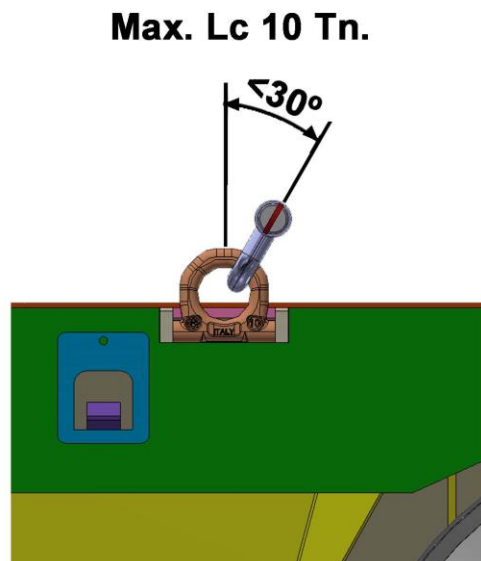
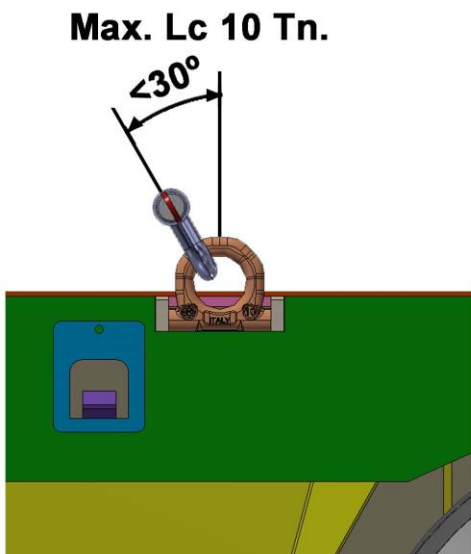
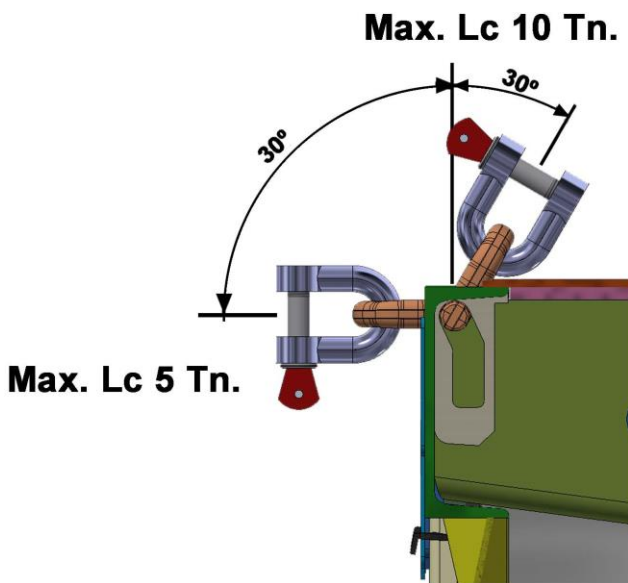
Lateral loads are forbidden in the neck


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Respect max load of lashing rings. Check ring registration (fig. 4.2)

Fig. 4.2 Lashing rings



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4.3.- LIFTABLE FRONT AXLE (OPTIONAL)

Control of lifting axles is automatic and depends on the load carried. Switch (fig. 4.3) is used to disconnect the automatic lift exceptionally.



Switch has to be **ALWAYS** in “I” position (fig. 4.4).



Disconnect switch (position “0”) **ONLY** in exceptional cases, out of public roads.



Fig 4.3 Lifting axle switch



4.4.- UP / DOWN LOADING PLATFORM



Driving out of ride height is only permitted up to 10 Km/h just to avoid some obstacles on the road, but only occasionally.

Loading Platform can go up and down using the Up&Down pneumatic valve (fig. 4.5).

- Press and turn levers right and left to raise or lower platform height.
- Push out the lever to return to ride height.



Once we pass the obstacle, immediately return to ride height pressing out the lever.

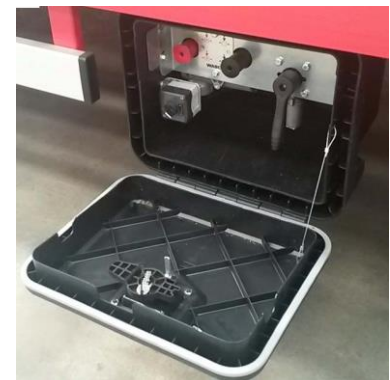


Fig. 4.5 Oscillating Neck button pad

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4.5.- UP / DOWN CENTRAL RAMP (OPTION)



It is forbidden to remain below or near the ramps during this operation. Danger of crushing people or objects.

The central ramp can only be lowered with load on it.

The central ramp are operated by lever # 4 of the Distributor + the control panel, which activates the pump (fig 4.6 and fig 4.7).

- Follow stickers instructions.



Fig. 4.6 Distributor ramp situation



Fig. 4.7 ramps control panel and levers



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4.8.- UP / DOWN RAMPS (OPTIONAL)



It is forbidden to stay under or near the ramps during this operation. Risk of crushing damages on persons or objects if ramps fall down.



Before operating ramps, release security cable on the ramps (fig. 4.22) and lower ramp supports (fig. 4.23).

Ramps can be operated by means of distributor levers + a power button (fig. 4.24).

Lever 1 (fig.4.24) is used to up and down ramps.

Lever 2 (fig.4.24) is optional for left ramp side displacement.

Lever 3 (fig.4.24) is optional for right ramp side displacement.

Power button 5 (fig.4.24) is used to switch on the pump.

- Follow stick indications.



Once finished this operation, fasten ramps with security cable (fig. 4.22) and upload ramp supports (fig.4.23).



Fig. 4.8 Security cable ramps



Fig. 4.9 ramps distributor situation



Fig. 4.10 ramps control panel and levers

4.9.- SELF-STEERING AXLE (OPTIONAL)

Self-steering axle is automatically blocked when driving backward.



Check axle blocking before driving backward.

It is also automatically blocked when driving forward above 40 Km/h and released under 35 Km/h.

However, you can block it permanently driving forward and backward by means of manual switch (fig. 4.25) in position "I" (fig. 4.26)



To drive with self-steering axle disconnect blocking switch (position "0").

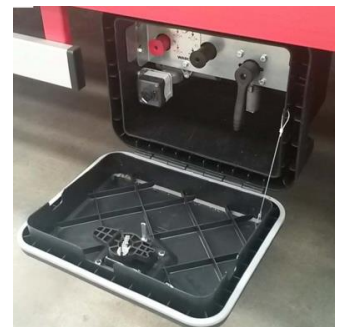



Fig. 4.11 self steering axle blocking switch



Fig. 4.12 switch in "I" position

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5.- MAINTENANCE

5.1.- MAINTENANCE SCHEDULE

Every day: visual inspection of the wheel nuts, tire pressure and wear, operation of the electrical system and the ABS or EBS system, this system gives the vehicle more active safety, more braking efficiency.

At 1000 km:

-check tightening of the wheel nuts.

This check must be performed each time a wheel is removed. The tightening torque should be between 600 and 700 Nm.

-Retight the suspension and re-align the axles (Annex 1)

Every month: change the grease of the disc of the 5th wheel after cleaning the disc and the KP, of all the elements of the vehicle to grease to ensure the good operation.

Every 3 months: check the condition of the brake pads, for disc brakes, this warning light is located on the brake caliper, as indicated on the axle maintenance manual. For drum brakes, the warning light is located on the automatic brake levers.

Every 6 months: re-tighten the suspension elements and clean the valve filters.

Once a year: Check the joints, and also tighten the suspension to ensure proper operation and re-align axles (Annex 1), also clean the valve filters.

These intervals have been made for normal use of the vehicle, reason why, these intervals are shortened in case of using the vehicle in very hard conditions.

In addition to the basic reviews, a thorough inspection is recommended before each trip or suspected of malfunction.

5.2- Particularities of maintenance operations


GREASE:

- Before greasing, clean the dust, water and mud.
- Inject grease until the lubrication point suppresses clean grease.
- Do not mix oils of different brands or different specifications.

The vehicle leaves the factory with "AGRIP GR MV / EP 2" grease (lytic grease). Use the same or equivalent.

OIL TANK

To check the oil level, the vehicle must be horizontal and the oil must be cool. If you want to add the oil, ensure the cover and general area cleanliness and use of hydraulic oil according to the following basic data (for other specifications, consult the manufacturer).

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GENERAL NOTICES ON USE OF HYDRAULIC OILS

- Never mix the oils.
- A good oil, of good quality, ensures a good lubrication and durability of the elements.
- The oil tank must remain closed; The air is sucked by means of a filling cap with the corresponding filter
- Keep the return filters clean.
- Verify oil level, it should contain 35% to 40% more than the working volume of the cylinders.
- The oil change must be done by experts. Please save the extracted oil and dispose of it according to environmental regulations.

HYDRAULIC STRUCTURES:

Check for leaks.

STICK AND LIGHTING DEVICES:

Check the cleaning of the stickers and lights.

WHEELS AND TIRES:

Check the condition of the tires, circulation belt, sidewalls and make sure there are no cracks.

Check the tire pressure.

CLEANING:

- Wash the trailer and remove any remaining dirt and adhering materials.

AXLES:

- Follow the instructions in annex 1 and the detailed instructions in the supplier's maintenance manuals.

They can be downloaded from:

Gigant: <http://www.gigant-group.com>

ROR: <http://www.rorcare.net>

SAF: [http:// safholland.com](http://safholland.com)

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PAY ATTENTION TO MAINTENANCE, GREASING,..ETC. OVER THE AXLES. THIS IS SHOWN IN THE MAINTENANCE BOOK WE ENCLOSE.



IT IS IMPORTANT TO DO FIRST MAINTENANCE OVER ALL BOLTS OF FIXATIONS AND BOLTS, RIGHT AFTER FIRST TRANSPORT WITH LOAD ON THE SEMITRAILER AND 1.000KM TRIP..



90% OF PROBLEMS ON AXLES AND SUSPENSIONS ARE ORIGINATED BY LACK OF TRACKING AND FOLLOW UP OF THESE INSTRUCTIONS. NOT FOLLOWING THE INSTRUCTIONS IT WILL CONSIDER A USE MALFUNCTION, AND WILL FORCE TO VOID WARRANTY FOR THE UNIT.



IMPORTANT: AFTER 1ST CHECK-UP, THIS PAPER SHEET HAS TO BE FILLED IN AND SIGNED, AND SEND EMAIL TO : *service@rojotrailer.com*, ATTACHING THE TECHNICAL DATA SHEET.

IN CASE OF DOUBT OR IF YOU NEED ANY FURTHER INFORMATION ABOUT MAINTENANCE, PLEASE CONTACT OUR AFTER-SALES DEPARTMENT Tlf. +34 947 54 64 11.

COMPANY		VAT	
ADDRESS		CITY	
		COUNTRY	
TELEPHONE			
UNIT INFO		WARRANT DATE	
MODEL		O.P.	
CHASSIS NUMBER			

DAY	WORK DONE	
WPRKSHOP		STAMP


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DIAGRAMS OF LIGHTS AND CONNECTIONS TO THE TRUCK:

General outline of electrical lighting installation:
Drawing 6M0D52416LED

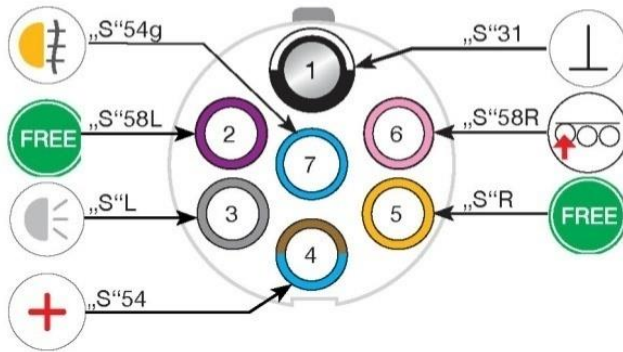
Connections to the truck:

SYMBOL LEGEND						SYMBOL LEGEND					
Symbol											Symbol
	Masse	Earth	massa	Masse	Masa	Terra	Massa	Jord	jord	maa	
	Blinker links	LH rear direction indicator	freccia sinistra	Clignotant gauche	Intermitente izquierdo	Pisca esquerdo	knipperlicht Links	Venstre blinklys	blinker vänster	vilkku vasen	
	Blinker rechts	RH rear direction indicator	freccia destra	Clignotant droit	Intermitente derecho	Pisca direito	knipperlicht Rechts	Høyre blinklys	blinker höger	vilkku oikea	
	Bremslicht	stop	stop	Feu stop	Luz de freno	Freio	Rem licht	Bremselys	bromslyjus	jarruvalo	
	Standlicht links	LH Tail	posizione sinistra	Feu de position gauche	Luz posición izquierda	Luz de posição esquerda	Achterlamp Links	Venstre parklys	parktjus vänster	parkki vasen	
	Standlicht rechts	RH Tail	posizione destra	Feu de position droit	Luz posición derecha	Luz de posição direita	Achterlamp Rechts	Høyre parklys	parktjus höger	parkki oikea	
	Nebelschlussleuchte	rear fog	retronebbia	Feu de brouillard	Luz antiniebla	Luz de neblina	Mist lamp	Tåkelys	dimijus	sumuvalo	
	Rückfahrleuchte	reverse	retromarcia	Feu de recul	Luz marcha atrás	Luz de Ré	Achteruitrij lamp	Baklys	backljus	peruutusvalo	
	All. Stromversorgung (+)	permanent power supply	positivo permanente	Alimentation électrique permanente (+)	corriente continua	corrente continua	Constante stroomvoorzorging	Allmenn strømforsyning	allmann strömförsörjning	jatkuva virta	
	Achselanhebung	axle lift	Sollevatore	Relevage d'essieu	dispositivo de elevación del eje	dispositivo de elevação do eixo	Aslift	Akselloft	axellyft	akselinosto	
	Federspeicheranzeige	braking control	Controllo funzionamento freni	Contrôle freinage remorque	Control de frenos para trailers	Controle de funcionamento dos freios	Remcilinder markering	Bremseindikator	bromsindikator	jousijarrun ilmaisin	
	Verschleißanzeige	brake pad wear Indicator	Controllo usura pastiglie	Témoin d'usure des plaquettes de frein	Indicador de desgaste de pastillas de freno	Indicador de desgaste de pastilhas de freio	Remslitage indicator	Siltasjeindikator	siltageindikator	jarrujen kulumisen ilmaisin	
	Masse für Elektroniken	Earth - Data	Massa per elettronica	Masse des éléments électroniques	masa funciones electrónicas	Terra para funções eletrônicas	Massa Data	Jord for elektronikk	jord för elektronik	elektroniikan maa	
	freie Wahl	unallocated	libero	Libre	Libre elección	Libre	Vrije positie	Fritt valg	fritt val	vapaa	

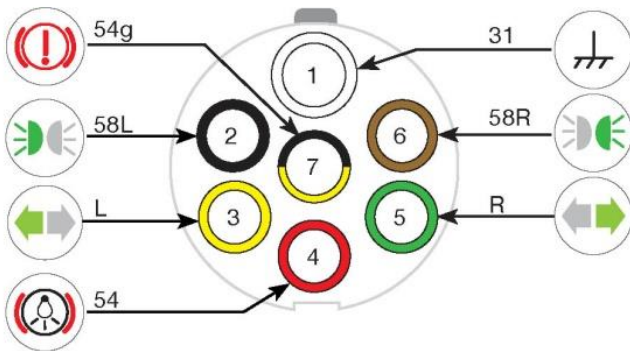
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ISO 3731
7 pin



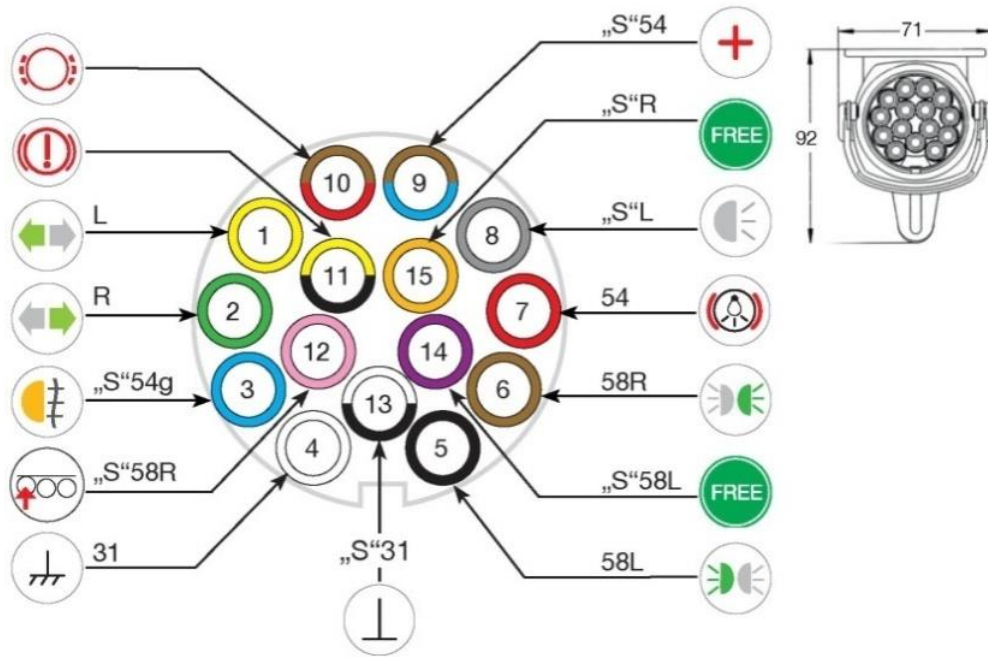
ISO 1185
7 pin




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ISO
 12098
 15 pin



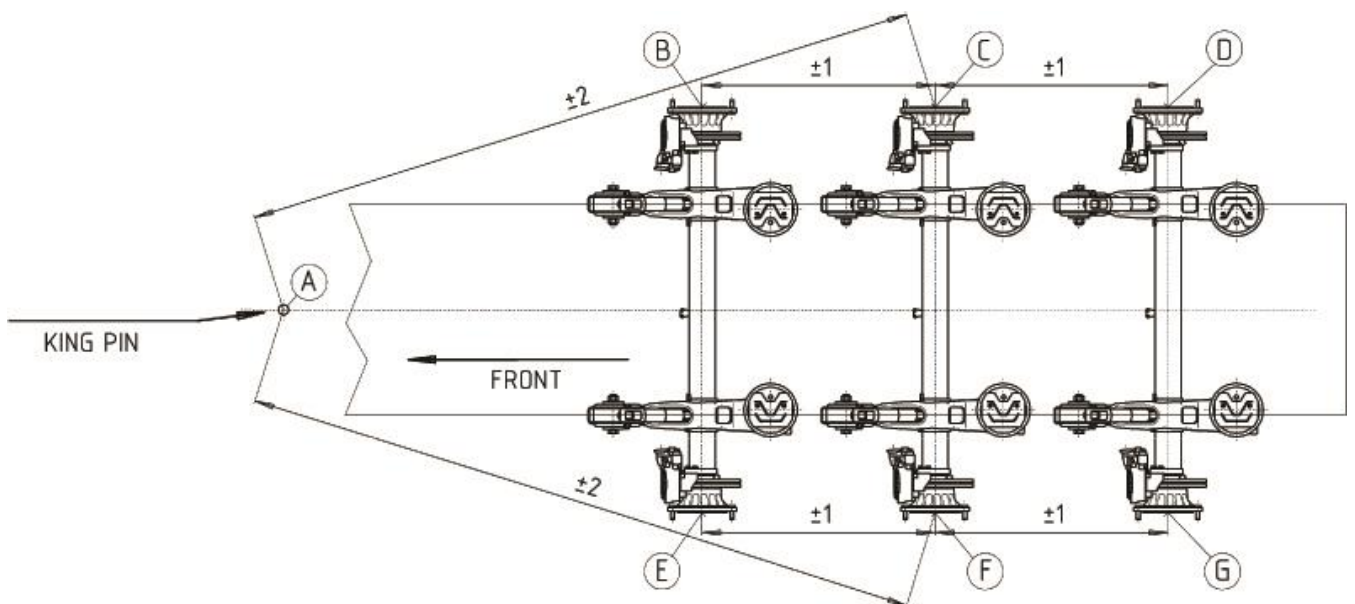
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ANEXO 1

Realizar siempre a los 1000 km. y cada año.

Determine the lengths of the diagonals **A - C** and **A - F** for the middle axle(reference axle) by comparison measurements, observing the tolerances(± 2 mm).

Check the wheelbases **B - C** and **E - F** for the front axle and **C - D** and **F - G** for the rear axle and correct, if necessary, observing the tolerances(± 1 mm).



**(5) AT 1000 km.(*)
EACH YEAR.
ROR. Tightening torque 70 Nm
SAF. Tightening torque 40 Nm**

**(1) AT 1000 km.(*)
EACH YEAR.
ROR. Tightening torque 300 Nm
SAF. Tightening torque 400 Nm**



**(4)
AT 1000 km.(*)
EACH YEAR.
ROR. Tightening torque 180 Nm
SAF. Tightening torque 180 Nm**

**(2)
AT 1000 km.(*)
EACH YEAR.
ROR. Tightening torque 800 Nm
SAF. Tightening torque 580 Nm**

**(3)
AT 1000 km.(*)
EACH YEAR.
ROR. Tightening torque 1100 Nm
SAF. Tightening torque 1200 Nm**



**AT 150 AND 500 km.
EACH 3 MONTHS.
EACH TYRE CHANGING
FOLLOW PREVIOUS STEPS
Tightening torque 600 Nm**

(*) ES NECESARIO QUE SE HAYA REALIZADO ALGUN VIAJE CARGADO

SAF MODUL

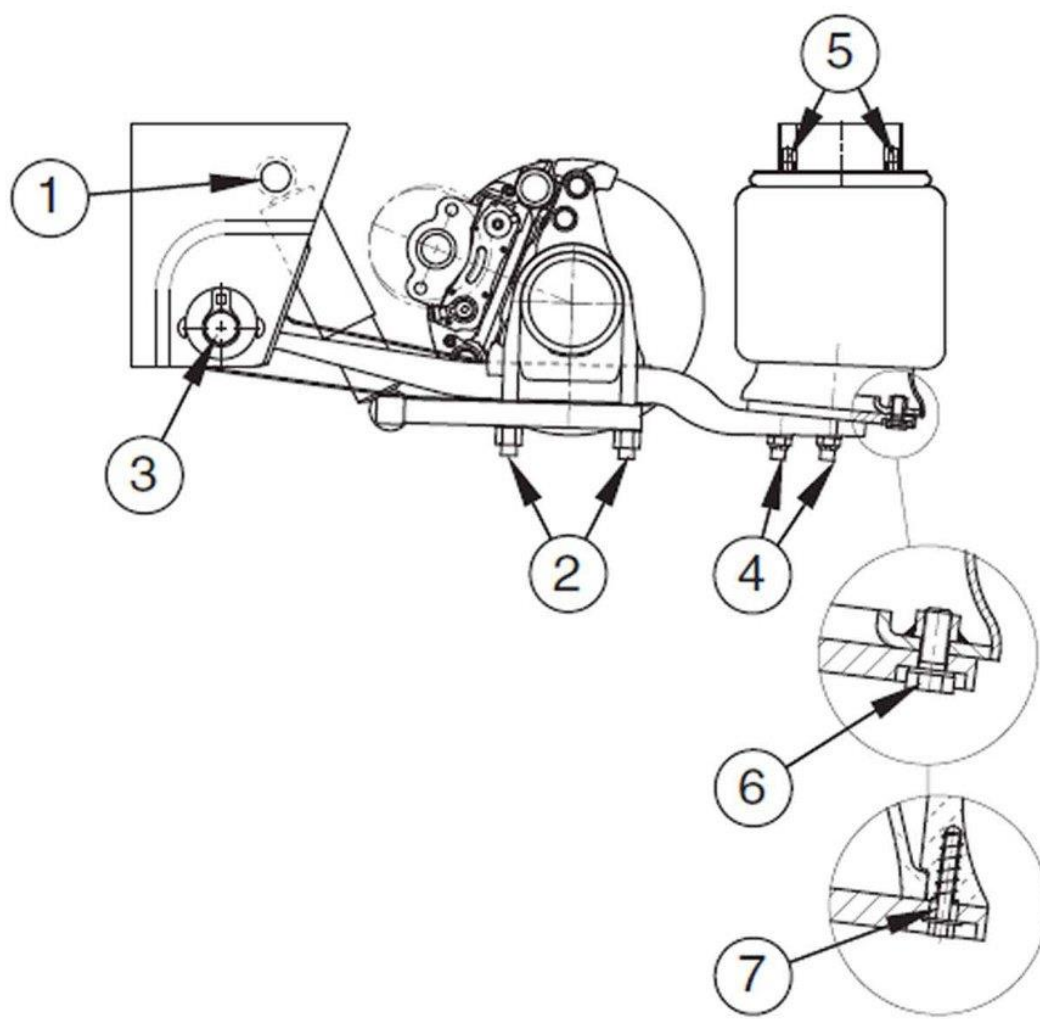
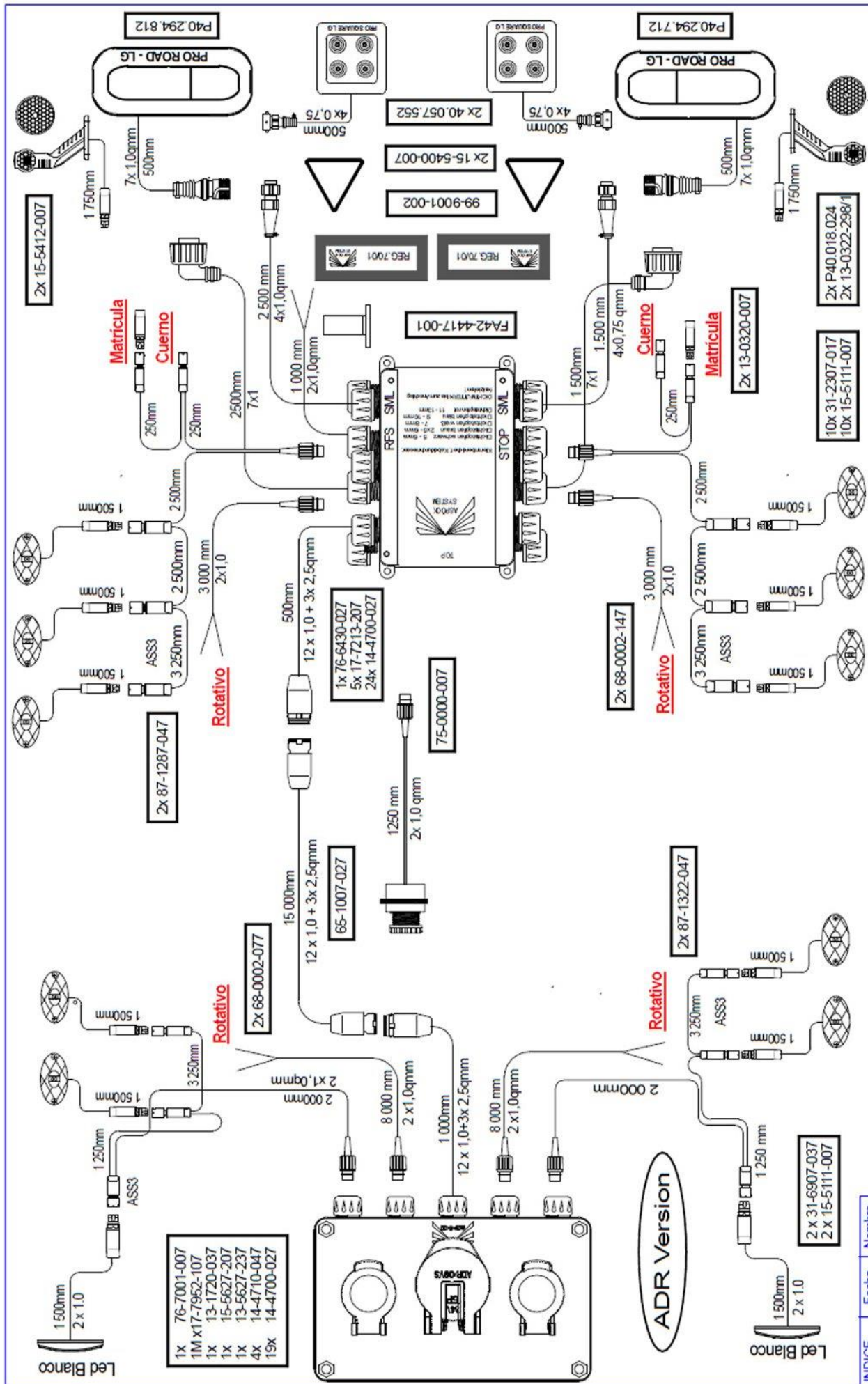


Fig. 12 · SAF MODUL inspection torque

Item.	Screw connection	Inspection torque (Nm)	Width across flats
-1-	M24x2	400	36
-2-	M22x1,5	580	32
-3-	M30	1200	46
-4-	M20	180	30
-5-	M12	40	19
-6- ²⁾	M12	80	19
-7- ³⁾	K100x40	20	10



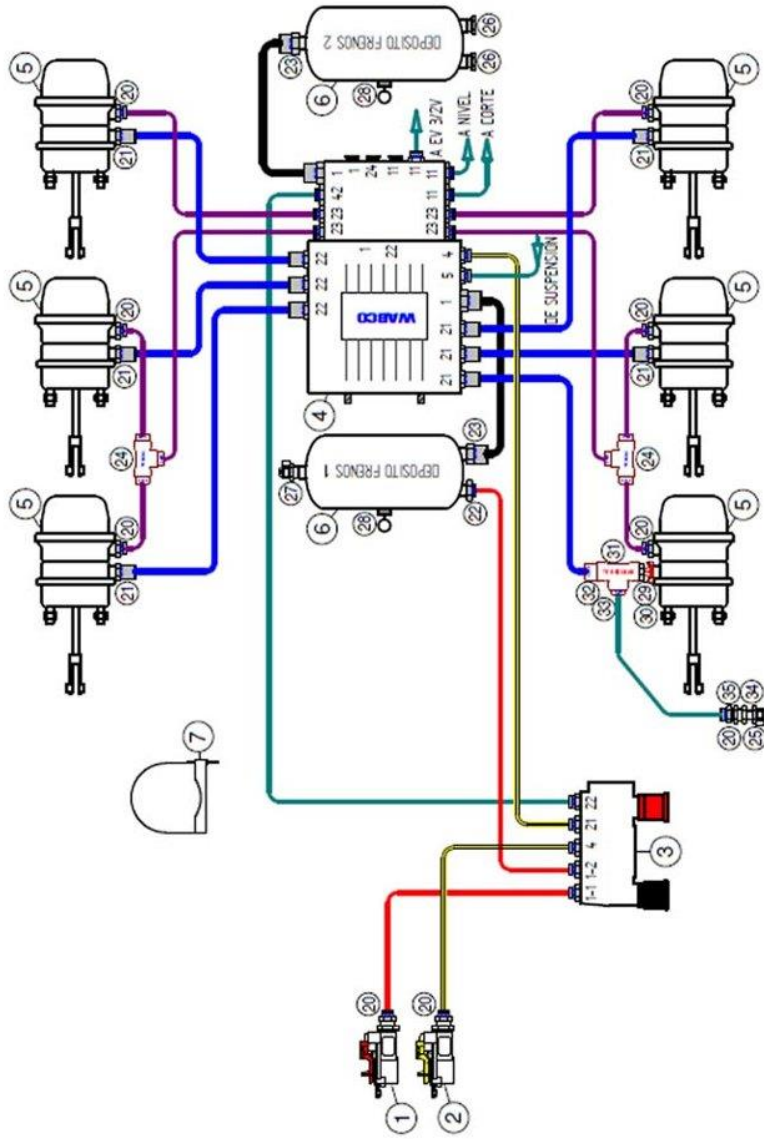
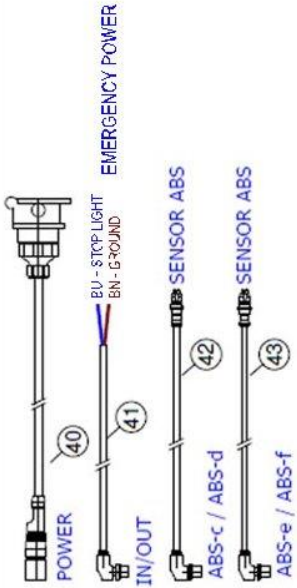
INDICE	Fecha	Nombre
Autor:	17.03.21	L.D
Modificado:		
Comentario:		

No. Plano: **21MOD524/16LED**

Cliente: **ROJO**



F. ASPÖCK
FAHRZEUGELEKTRIK



Marca Cant.	DENOMINACION	REFERENCIA
1	CABEZA ACOPL. R+FILTRO C/P	952 201 012 0
2	CABEZA ACOPL. A+FILTRO C/P	952 201 011 0
3	PREV C/RAC.	400 651 434 0
4	EBS E PREMIUM+PEM C/RAC.	400 651 931 0
5	ACTUADOR FRENO	A DEFINIR
6	DEPOSITO	A DEFINIR
7	ABRAZADERA DEPOSITO	A DEFINIR
20	RECTO M16-8/6 ABC	893 800 001 2
21	RECTO M16-12/9 ABC	893 800 002 2
22	RECTO M22-8/6 ABC	893 800 005 2
23	RECTO M22-15/12 ABC	893 800 004 2
24	TE AEREA Ø8/6 COMPOSITE	400 803 218 0
25	RACOR PRUEBA M16 M	463 703 120 0
26	TAPON M22 c/ J.T.	893 022 015 4
27	RACOR PRUEBA M22 M	463 703 115 0
28	GRIFO DE PURGA M22 C/ J.T.	934 300 038 0
29	UNION M16 ABC REGULAR	893 920 317 2
30	BLOQUEO 22 ABC REGULAR	899 700 116 4
31	TE ABC REGULAR	893 503 072 4
32	RECTO 12/9 ABC REGULAR	893 800 093 2
33	RECTO 8/6 ABC REGULAR	893 800 091 2
34	UMH M22-M16H L24	893 104 058 4
35	TUERCA M22	891 503 076 4
40	CABLE ALM. ISO 7P SEM. 12m	449 173 120 0
41	CABLE 24N 6m	449 349 060 0
42	CABLE SENSOR 3m - EBS E	449 723 030 0
43	CABLE SENSOR 5m - EBS E	449 723 050 0

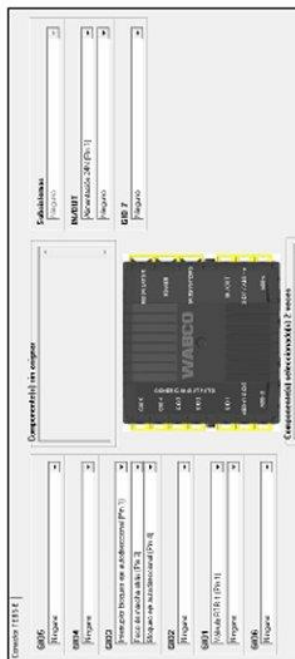
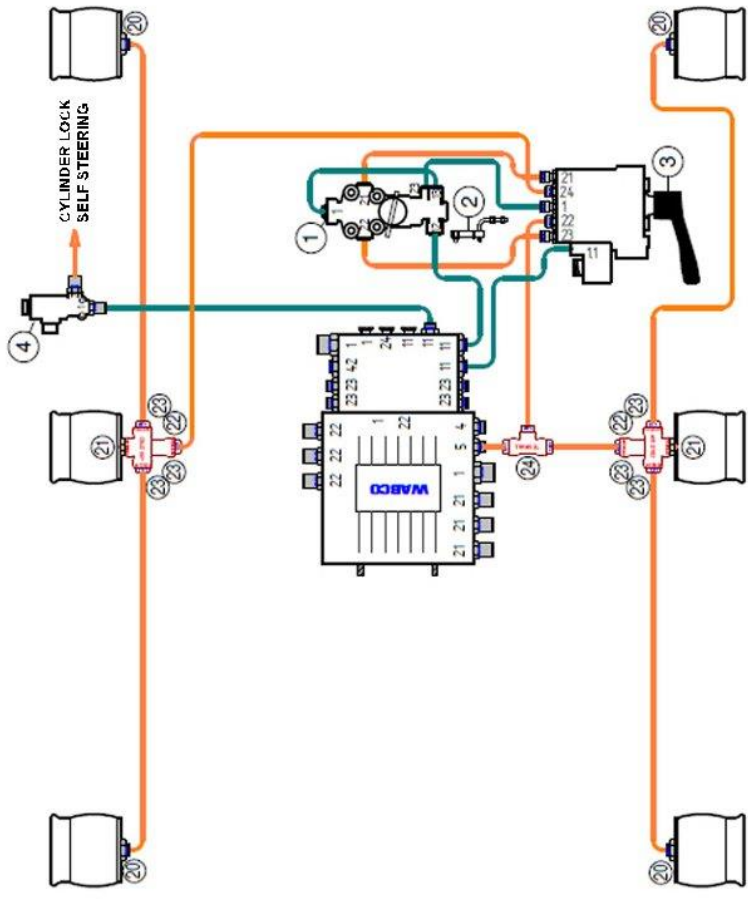
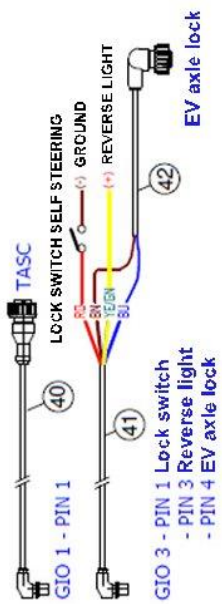
- TUBERIAS
- Ø 8/6
 - Ø 8/6
 - Ø 8/6
 - Ø 8/6
 - Ø 12/9
 - Ø 15/12



FOR SUSPENSION SCHEDULE SEE DTA-2455

Plano	DTA-2454		Conjunto	Brake System with EBS E + PEM	
SET del	400 60X XXX 0		Denominación	SEMITRAILER 3 EJES	
DIBUJADO	FECHA	NOMBRE			
	23-09-2015	MARTÍNEZ			





FOR BRAKE SCHEDULE SEE DTA-2454

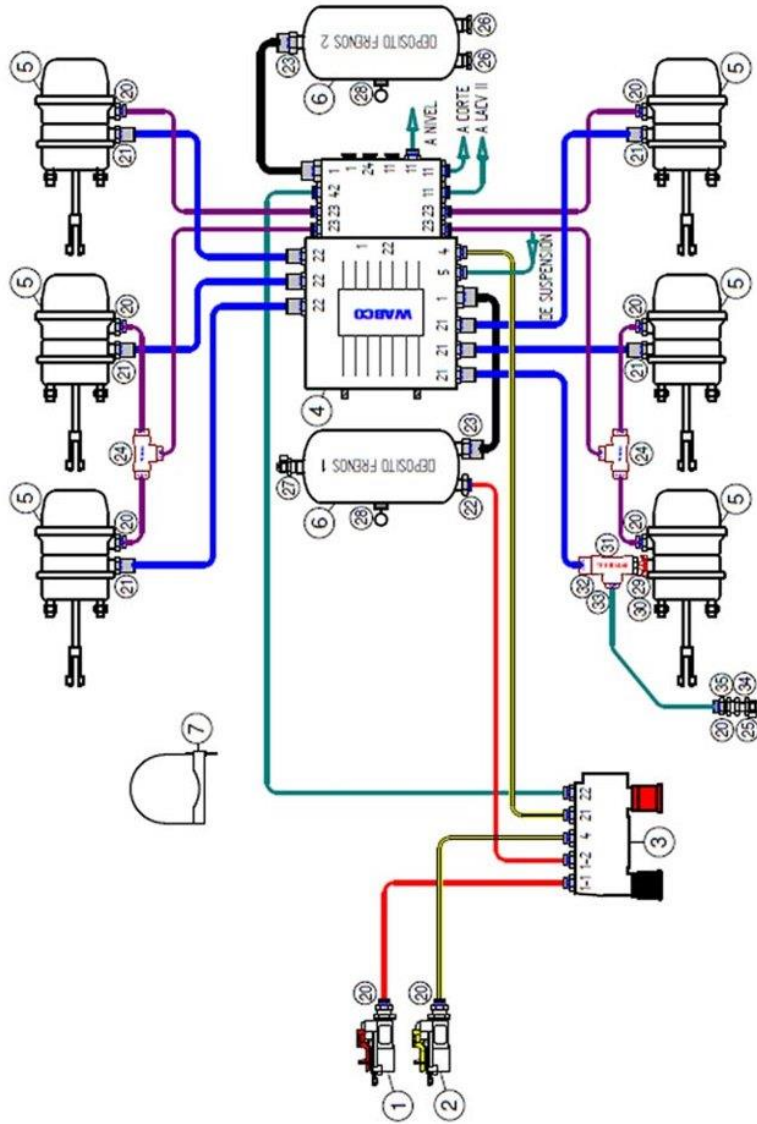
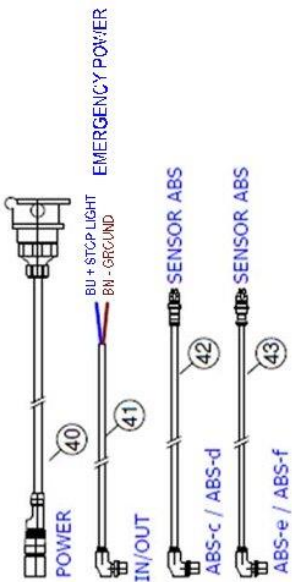
Marca	Cant.	DENOMINACION	REFERENCIA
1	1	NIVELADORA C/CORTE COMPOSITE C/RAC.	464 006 520 0
2	1	VARILLAS NIVELADORA	433 401 003 0
3	1	TASC para Corte c/RAC	463 090 311 0
4	1	EV 3/2V MC 24V C/RAC	400 650 404 0
		EV 3/2V MA 24V C/RAC	400 651 934 0
20	4	RECTO M22-8/6 ABC	893 800 005 2
21	2	UNION M22 ABC REGULAR	893 920 318 2
22	2	CRUZ 90° ABC REGULAR	893 905 801 4
23	6	RECTO 8/6 ABC REGULAR	893 800 091 2
24	1	TE AEREA 08/6 COMPOSITE	400 803 218 0
40	1	CABLE 6/0 - LACV III/TASC/ZUMBADOR 10m	449 443 100 0
41	1	CABLE 6/0 UNIVERSAL 6m	449 535 060 0
42	1	CABLE EV. ABS BAY-DIN C. 5m	449 515 050 0

HOSES
 Ø 8/6
 Ø 8/8

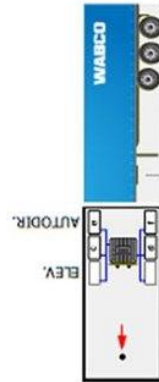
Modificación	Fecha	Nombre
A	15-12-2020	MARTÍNEZ

Tipo de modificación
 Conjunto
DTA-2455
 Suspensión equipment with fittings
 Denominación
 SEM. 3 AXLE. LEVEL C/C and TASC - with PEM
 del 400 60X XXX 0
 FECHA: NOMBRE
 23-09-2015 MARTÍNEZ





Marca	Cant.	DENOMINACION	REFERENCIA
1	1	CABEZA ACOPL. R+FILTRO C/P	952 201 012 0
2	1	CABEZA ACOPL. A+FILTRO C/P	952 201 011 0
3	1	PREV C/RAC.	400 651 434 0
4	1	EBS E PREMIUM+PEM C/RAC.	400 651 931 0
5	6	ACTUADOR FRENO	A DEFINIR
6	2	DEPOSITO	A DEFINIR
7	4	ABRAZADERA DEPOSITO	A DEFINIR
20	9	RECTO M16-8/6 ABC	893 800 001 2
21	5	RECTO M16-12/9 ABC	893 800 002 2
22	1	RECTO M22-8/6 ABC	893 800 005 2
23	2	RECTO M22-15/12 ABC	893 800 004 2
24	2	TE ABREA 08/6 COMPOSITE	400 803 218 0
25	1	RACOR PRUEBA M16 M	463 703 120 0
26	2	TAPON M22 C/J.T.	893 022 015 4
27	1	RACOR PRUEBA M22 M	463 703 115 0
28	2	GRIFO DE PURGA M22 C/J.T.	934 300 038 0
29	1	UNION M16 ABC REGULAR	893 920 317 2
30	1	BLOQUEO 22 ABC REGULAR	899 700 116 4
31	1	TE ABC REGULAR	893 503 072 4
32	1	RECTO 12/9 ABC REGULAR	893 800 093 2
33	1	RECTO 8/6 ABC REGULAR	893 800 091 2
34	1	UMH M22-M16H L24	893 104 058 4
35	1	TUERCA M22	891 503 076 4
40	1	CABLE ALIM. ISO 7P SEM. 12m	449 173 120 0
41	1	CABLE 24N 6m	449 349 060 0
42	2	CABLE SENSOR 3m - EBS E	449 723 030 0
43	2	CABLE SENSOR 5m - EBS E	449 723 050 0

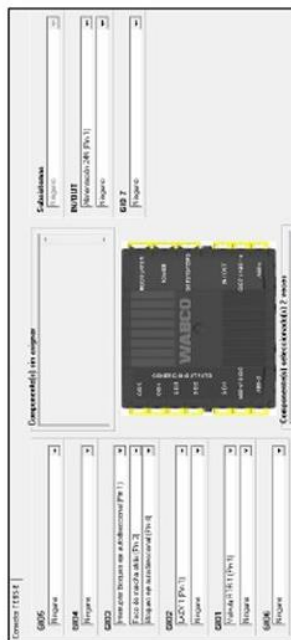
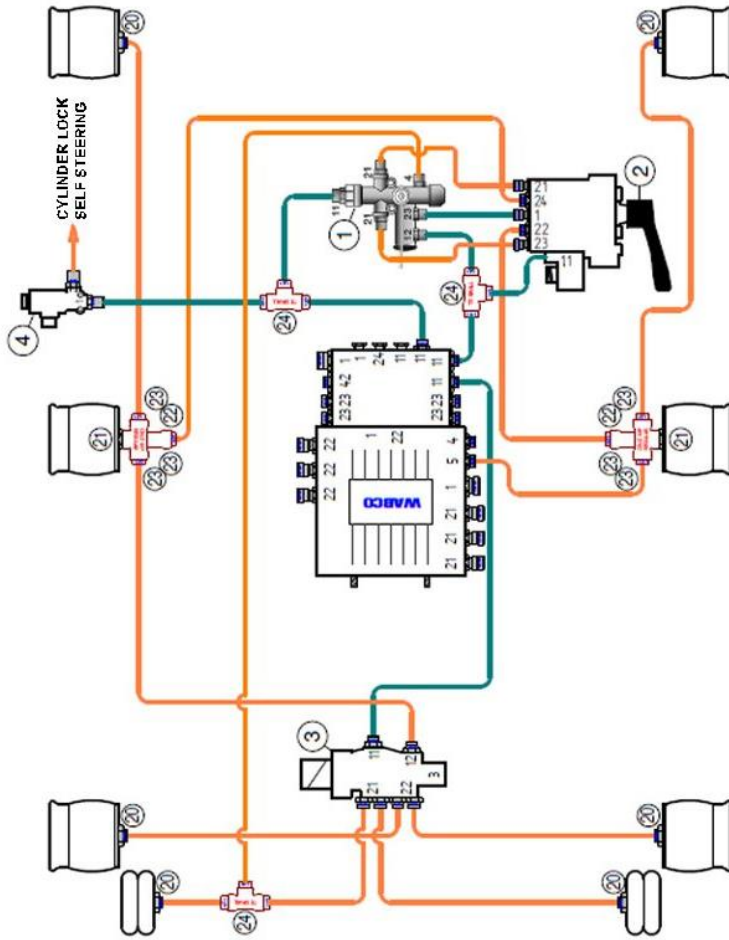
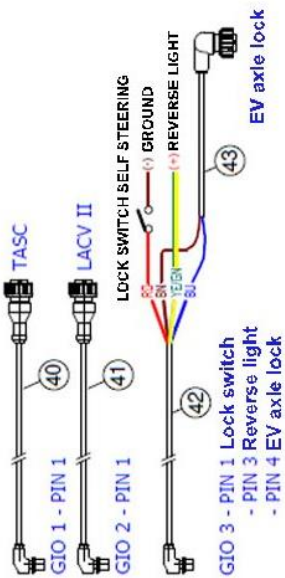


FOR SUSPENSION SCHEDULE SEE DTA-2388

- TUBERIAS
- Ø 8/6
 - Ø 8/6
 - Ø 8/6
 - Ø 8/6
 - Ø 12/9
 - Ø 15/12

Plano		Conjunto	
DTA-2387		Brake System with EBS E + PEM	
SET del 400 60X XXX 0		SEMIRAILER 3 EJES	
DIBUJADO	FECHA	NOBRE	
08-04-2015		MARTÍNEZ	



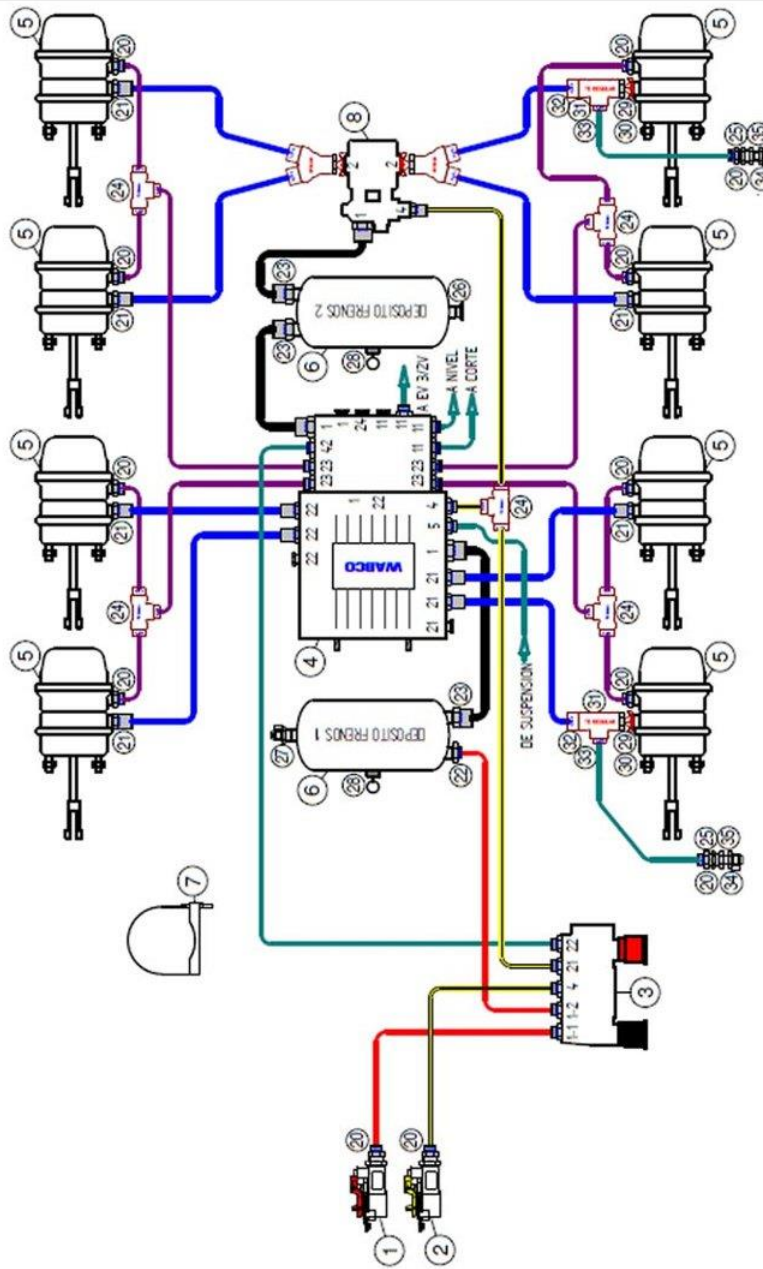
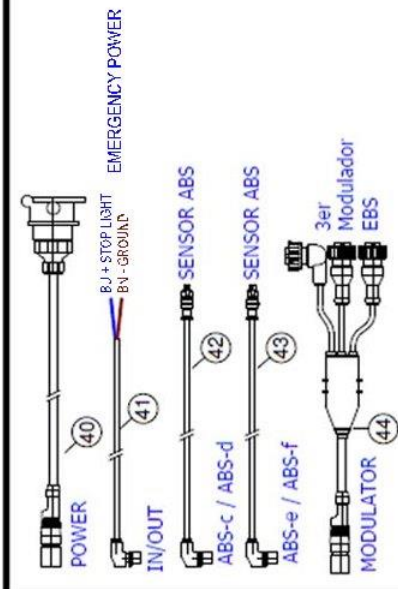


FOR BRAKE SCHEDULE SEE DTA-2387

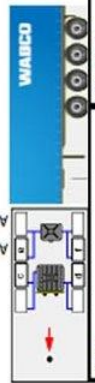
Modificación	C	09-10-2020	MARTÍNEZ	SUSTITUIDA TASC 400 651 745 0 POR 463 090 311 0 + TE AEREA
	B	05-01-2016	MARTÍNEZ	CORREGIDA REPRESENTACION Y CONEXIONADO DE NIVELADORA
	A	09-09-2015	MARTÍNEZ	SUSTITUIDA CONTROL ALTURA POR TASC Y AGREGADO SISTEMA SAC
Plano	Conjunto			
Denominación	Suspension equipment with fittings			
SEI del 400 60X XXX 0	SEMI: 3 AXLE: LEVEL C/C: TASC and LACV II - with PEM			
FECHA	08-04-2015	NOMBRE	MARTÍNEZ	
DIBUJADO	MARTÍNEZ			

HOSES
 Ø 8/6
 Ø 8/6

Marca	Cant.	DENOMINACION	REFERENCIA
	1	NIVELADORA C/ CORTE DOBLE ALTURA	NO WABCO
	2	TASC para Corte c/RAC.	463 090 311 0
	3	LACV II C/RAC.	400 651 277 0
	4	EV 3/2V MC 24V C/RAC.	400 650 404 0
	4	EV 3/2V MA 24V C/RAC.	400 651 934 0
	20	RECTO M22-8/6 ABC	893 800 005 2
	21	UNION M22 ABC REGULAR	893 920 318 2
	22	CRUZ 90º ABC REGULAR	893 905 801 4
	23	RECTO 8/6 ABC REGULAR	893 800 091 2
	24	TE AEREA Ø8/6 COMPOSITE	400 803 218 0
	40	CABLE G10 - LACV II/TASC/ZUMBADOR 10m	449 443 100 0
	41	CABLE G10 - LACV II/TASC/ZUMBADOR 4m	449 443 040 0
	42	CABLE G10 UNIVERSAL 6m	449 535 060 0
	43	CABLE EV. ABS BAY-DIN C. 5m	449 515 050 0



FOR SUSPENSION SCHEDULE SEE DTA-2463



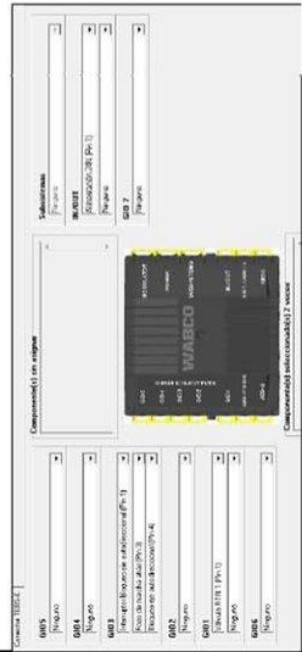
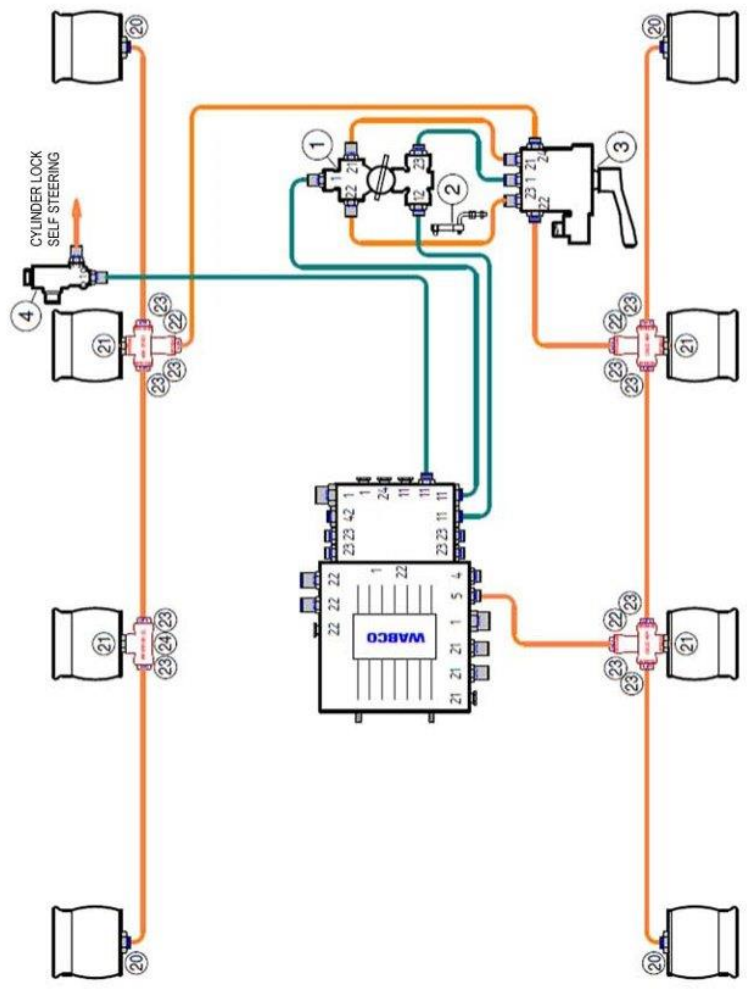
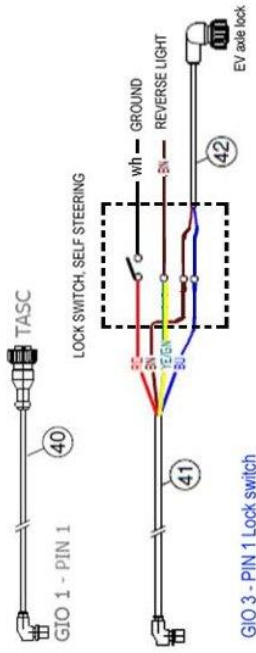
- TUBERIAS
- Ø 8/6
 - Ø 8/6
 - Ø 8/6
 - Ø 8/6
 - Ø 12/9
 - Ø 15/12

Brake System with EBS E + PEM

Denominación SEMITRAILER 4 EJES



DIBUJADO	07-10-2015	MARTÍNEZ
FECHA	NOMBRE	



FOR BRAKE SCHEDULE SEE DTA-2462

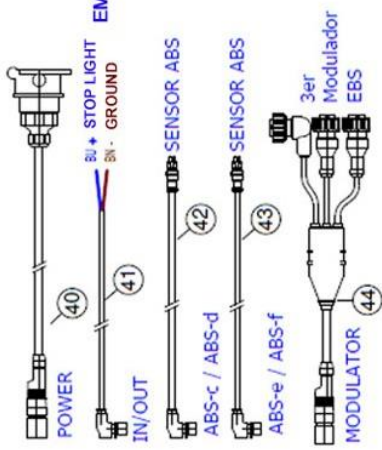
Marca	Cant.	DENOMINACION	REFERENCIA
1	1	NIVELADORA C/ CORTE C/RAC.	400 803 456 0
2	1	VARILLAJE NIVELADORA	433 401 003 0
3	1	TASC C/RAC.	400 651 745 0
4	1	EV 3/2V AC 24V C/RAC.	400 650 404 0
20	4	RECTO M22-8/6 ABC	400 651 934 0
21	4	UNION M22 ABC REGULAR	893 800 005 2
22	3	CRUZ 900 ABC REGULAR	893 920 318 2
23	11	RECTO 8/6 ABC REGULAR	893 905 801 4
24	1	TE ABC REGULAR	893 800 091 2
40	1	CABLE GIO - LACV III/TASC/ZUMBADOR. 10m	893 503 072 4
41	1	CABLE GIO UNIVERSAL 6m	449 443 100 0
42	1	CABLE EV. ABS BAY-DIN C. 5m	449 535 060 0
			449 515 050 0

A ELEGIR

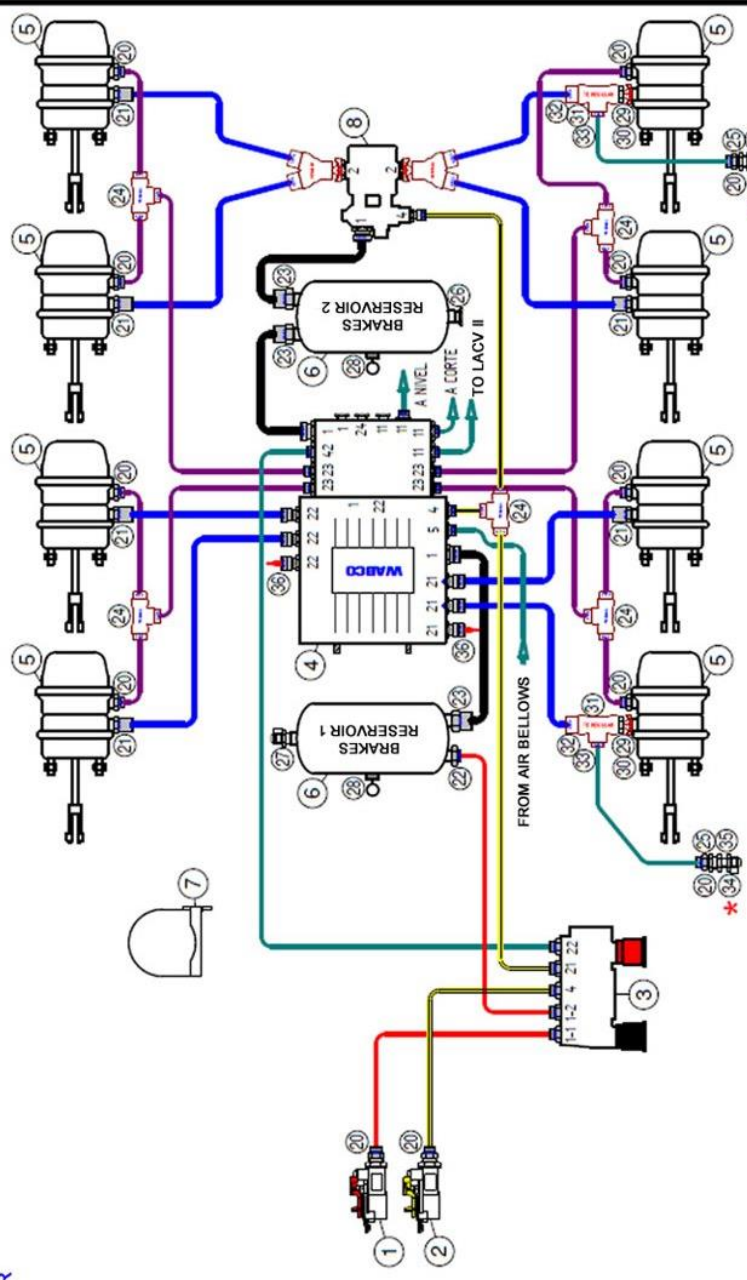
Modificación		Tipo de modificación	
Num.	Fecha	Nombre	
Plano			
DTA-2463		Conjunto	
Suspension equipment with fittings			
Denominación			
SET del 400 60X XXX 0		SEM. 4 axle , LEVEL C/C and TASC- with PEM	
FECHA		NOMBRE	
DIBUJADO	07-10-2015	MARTÍNEZ	

TUBERIAS
 — Ø 8/6
 — Ø 8/6

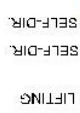




EMERGENCY POWER



* THIS TEST FITTING WILL BE MOUNTED IN THE MOST UNFAVORABLE CHAMBER IN RESPONSE TIMES



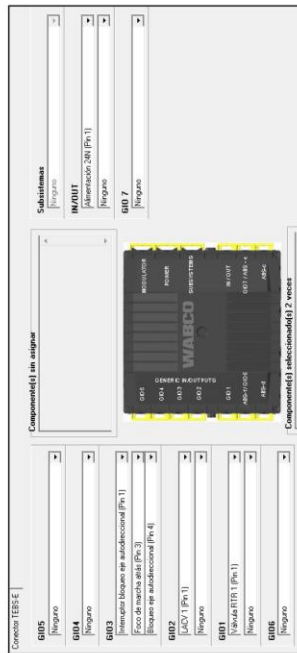
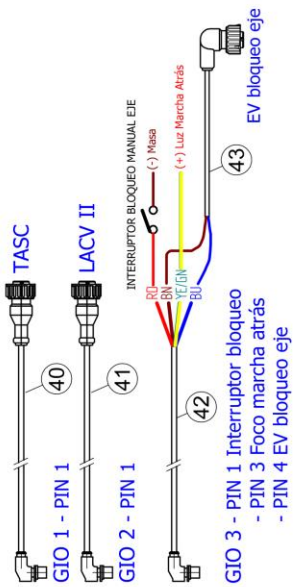
FOR SUSPENSION SCHEDULE SEE DTA-2398

Modificación	B	16-12-2020	MARTINEZ	ADUDICADAS REFERENCIAS A MARCAS 4 Y 8
	A	06-10-2015	MARTINEZ	MODIFICADA SENSORIZACION. AHORA 3º Y 4º AUTODIRECCIONALES
Num	Nombre			
Fecha	Tipo de modificación			
Plano				
DTA-2397				
Conjunto				
Brake System with EBS E + PEM				
Denominación				
SEMITRAILER 4 AXLE				
SET del 400 60X XXX 0				
DIBUJADO				
FECHA	14-04-2015	NOMBRE	MARTINEZ	

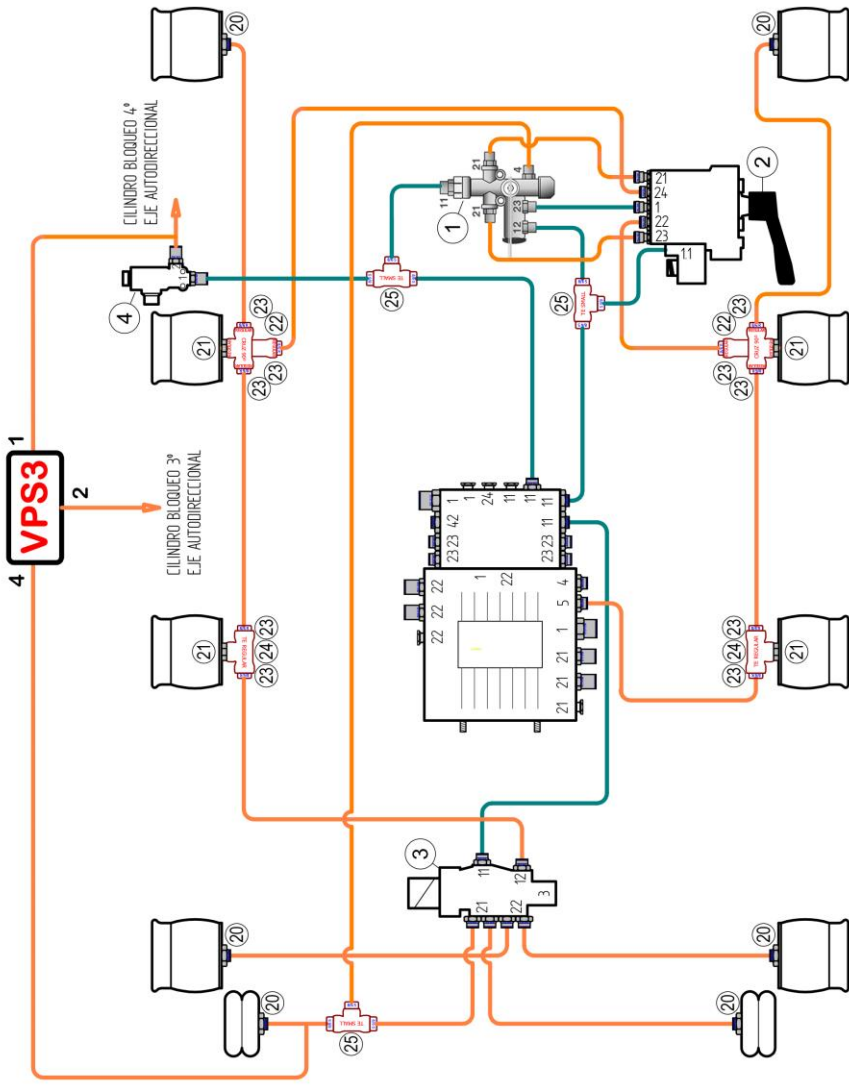
HOSES

- Ø 8/6
- Ø 8/6
- Ø 8/6
- Ø 8/6
- Ø 12/9
- Ø 15/12

Marca	Cant.	DENOMINACION	REFERENCIA
	1	CABEZA ACOPL. R + FILTRO C/P	952 201 012 0
	1	CABEZA ACOPL. A + FILTRO C/P	952 201 011 0
	1	PREV C/RAC.	400 651 434 0
	4	EBS E PREMIUM + PEM C/RAC.	400 651 931 0
	5	A DEFINIR	
	2	ACTUADOR FRENO	
	6	DEPOSITO	
	7	4 ABRAZADERA DEPOSITO	
	8	1 3ER MODULADOR EBS C/RAC.	400 652 470 0
	20	12 RECTO M16-8/6 ABC	893 800 001 2
	21	6 RECTO M16-12/9 ABC	893 800 002 2
	22	1 RECTO M22-8/6 ABC	893 800 005 2
	23	3 RECTO M22-15/12 ABC	893 800 004 2
	24	5 TE AEREA Ø8/6 COMPOSITE	400 803 218 0
	25	2 TUERCA M22	891 503 076 4
	26	1 TAPON M22 C/ J.T.	893 022 015 4
	27	1 RACOR PRUEBA M22 M	463 703 115 0
	28	2 GRUPO DE PURGA M22 C/ J.T.	934 300 038 0
	29	2 UNION M16 ABC REGULAR	893 920 317 2
	30	2 BLOQUEO 22 ABC REGULAR	899 700 116 4
	31	1 TE ABC REGULAR	893 503 072 4
	32	2 RECTO 12/9 ABC REGULAR	893 800 093 2
	33	2 RECTO 8/6 ABC REGULAR	893 800 091 2
	34	2 RACOR PRUEBA M16 M	463 703 120 0
	35	2 UNH M22-M16H L24	893 104 058 4
	36	2 TAPON 8 ABC	893 022 001 4
	40	1 CABLE ALUM. ISO 7P SEM. 12m	449 173 120 0
	41	1 CABLE 24N 6m	449 349 060 0
	42	2 CABLE SENSOR 3m - EBS E	449 723 030 0
	43	2 CABLE SENSOR 5m - EBS E	449 723 050 0
	44	1 CABLE 3ER MODULADOR EBS E 3m	449 429 030 0



Marca	Cant.	DENOMINACION	REFERENCIA
			NO WABCO
1	1	NIVELADORA C/ CORTE DOBLE ALTURA	463 090 311 0
2	1	TASC para Corte c/RAC	400 651 277 0
3	1	LACV II C/RAC.	400 650 404 0
4	1	EV 3/2V MC 24V C/RAC.	400 651 934 0
			A ELEGIR
20	6	RECTO M22-8/6 ABC	893 800 005 2
21	4	UNION M22 ABC REGULAR	893 920 318 2
22	2	CRUZ 90º ABC REGULAR	893 905 801 4
23	10	RECTO 8/6 ABC REGULAR	893 800 091 2
24	2	TE ABC REGULAR	893 503 072 4
25	3	TE AEREA Ø8/6 COMPOSITE	400 803 218 0
40	1	CABLE GIO - LACV II/TASC/ZUMBADOR 10m	449 443 100 0
41	1	CABLE GIO - LACV III/TASC/ZUMBADOR 4m	449 443 040 0
42	1	CABLE GIO UNIVERSAL 6m	449 535 060 0
43	1	CABLE EV. ABS BAY-DIN C. 5m	449 515 050 0



PARA ESQUEMA DE FRENOS VER **DTA-2397**

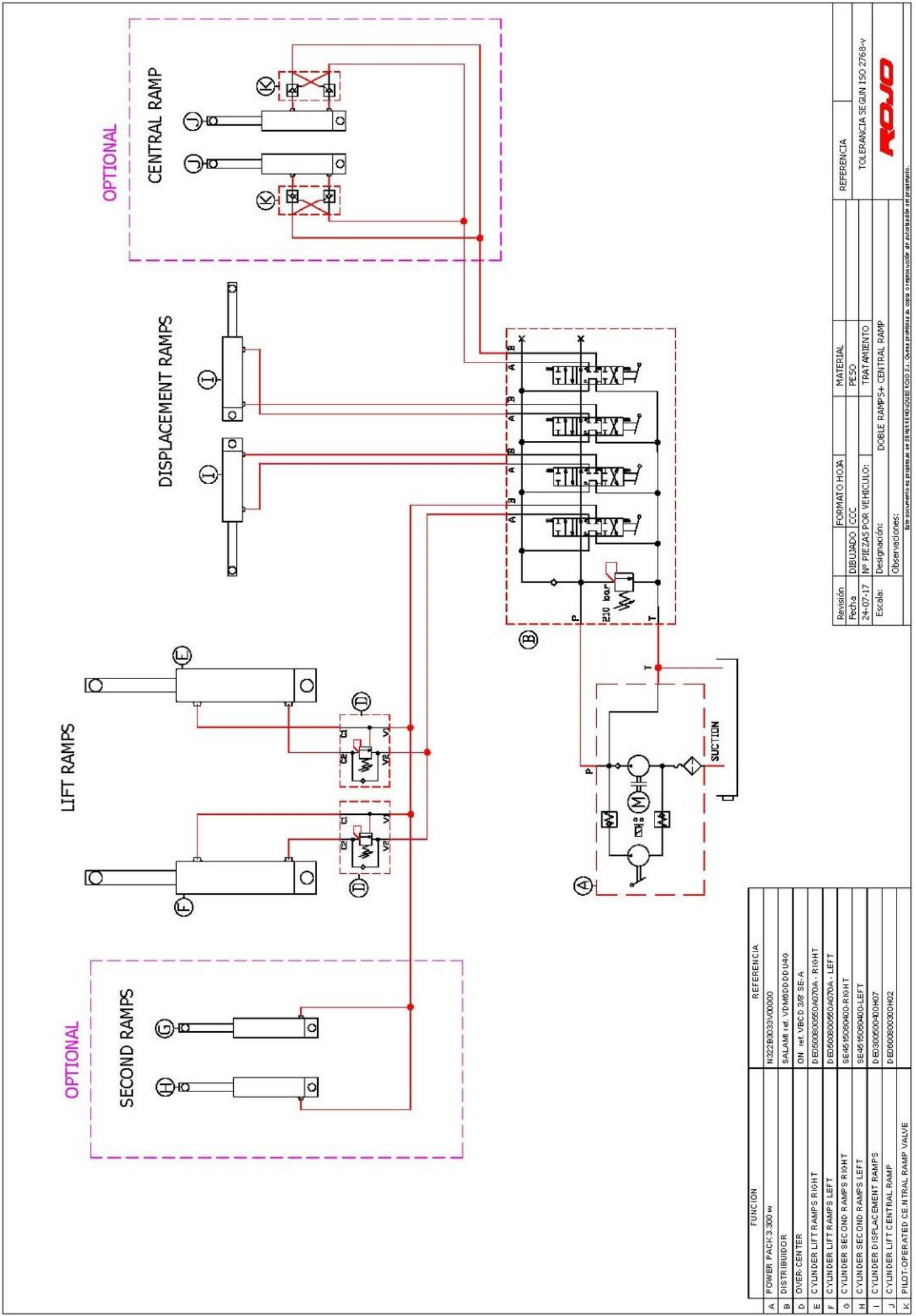
Modificación	Num.	Fecha	Nombre	Tipo de modificación
C	19-09-2023	ROIOTRAILER	VALVULA VEPSS	
B	14-12-2020	MARTINEZ	SUSTITUIDA TASC POR NUEVA TASC PARA CORTE.	
A	06-10-2015	MARTINEZ	SUSTITUIDA CONTROL ALTURA POR TASC Y AGREGADO SISTEMA SAC	

Plano	Conjunto
DTA-2398-C-R01	Equipo de Suspensión con racores
SET	Denominación
del 400 60X XXX 0	SEM. 4 EJES, NIVEL. C/C, TASC Y LACV II - CON PEM

DIBUJADO	FECHA	NOMBRE
DIBUJADO	18-04-2015	MARTINEZ

TUBERIAS
 Ø 8/6
 Ø 8/6

WABCO

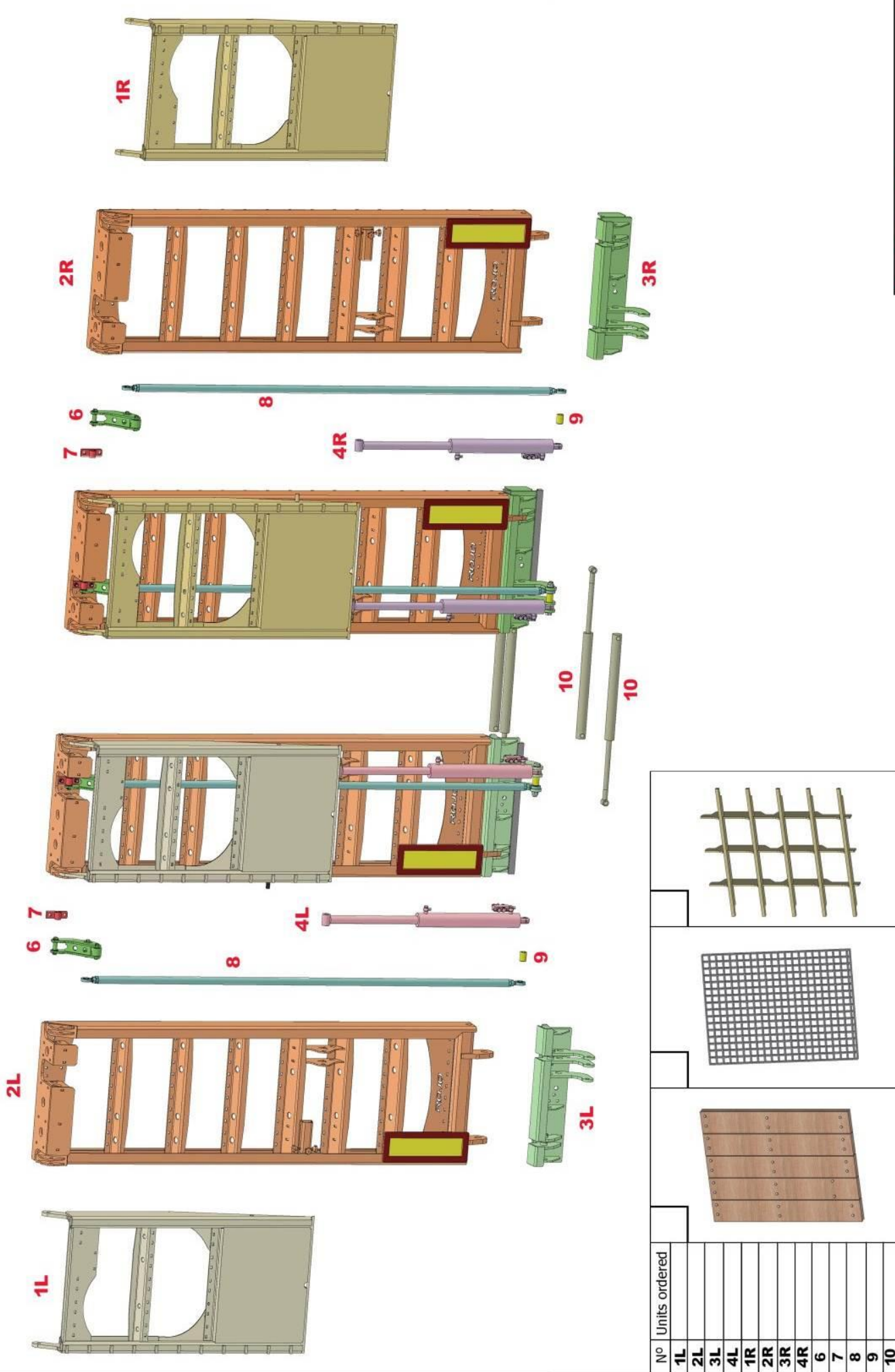


FUNCION	REFERENCIA
A POWER PACK 3.300 w	N322B033V00000
B DISTRIBUIDOR	SALAMI ref. VDM6DDDDU4G
D OVER-CENTER	DN ref. VBCD 3/8 SE-A
E CYLINDER LIFT RAMPS RIGHT	D-ED00080050A070A - RIGHT
F CYLINDER LIFT RAMPS LEFT	D-ED00080050A070A - LEFT
G CYLINDER SECOND RAMPS RIGHT	SE45 50060400-RIGHT
H CYLINDER SECOND RAMPS LEFT	SE45 50060400-LEFT
I CYLINDER DISPLACEMENT RAMPS	D-ED300600400H07
J CYLINDER LIFT CENTRAL RAMP	D-ED000800300H02
K PILOT-OPERATED CENTRAL RAMP VALVE	

Revisión:	FORMATO HOJA:	MATERIAL:	REFERENCIA:
Fecha:	CCC	PESO:	TOLERANCIA SEGUN ISO 2768-y
24-07-17	Nº PIEZAS POR VEHICULO:	TRATAMIENTO:	
Escala:	Designación:	DOBLE RAMPS+ CENTRAL RAMP	
	Observaciones:		

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N°	Units ordered
1L	
2L	
3L	
4L	
1R	
2R	
3R	
4R	
6	
7	
8	
9	
10	

Trailer VIN Number: **V S 9** _ _ _ _ **G S** _ **A 8 6 9** _ _ _

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